

# BAY AREA TOLL AUTHORITY

Regional Measure 1  
Toll Bridge Projects

## Project Monitoring Program

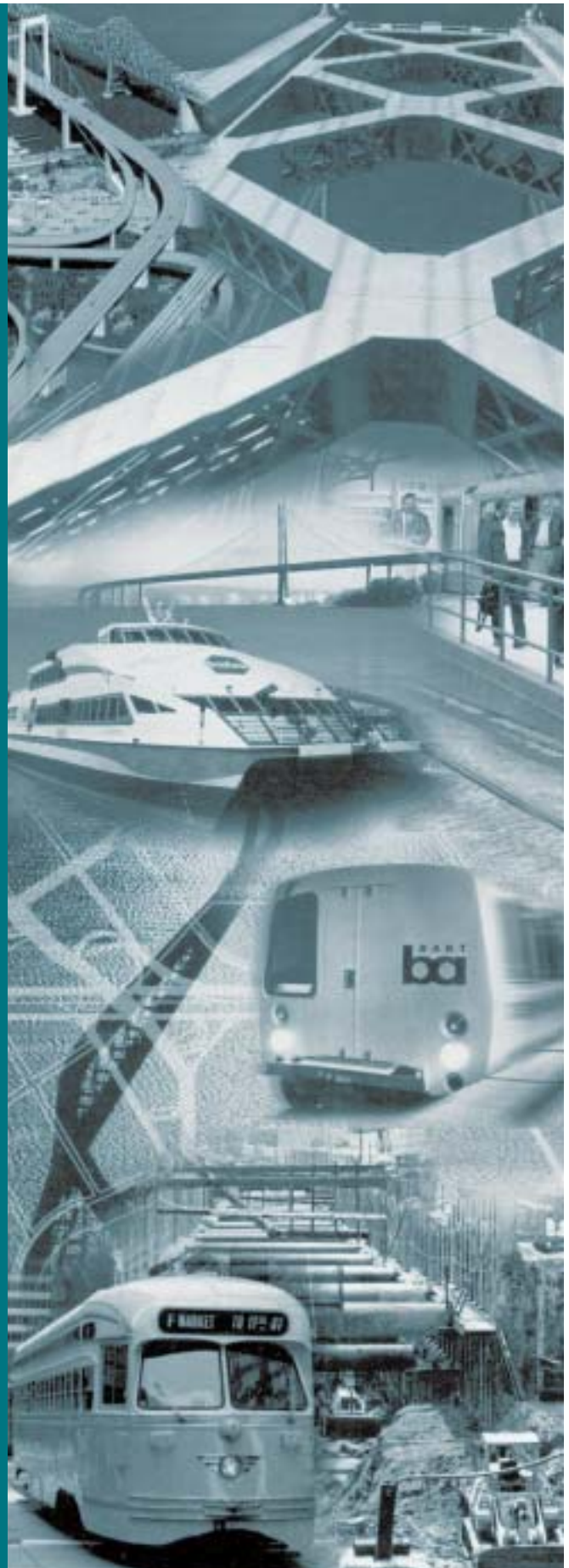
APRIL 2002 PROGRESS REPORT



Metropolitan Transportation  
Commission

Bay Area Toll Authority

Released May 2002



# Bay Area Toll Authority/Metropolitan Transportation Commission

## *Commission Roster*

Sharon J. Brown, MTC Chair  
Cities of Contra Costa County

Steve Kinsey, MTC Vice Chair  
Marin County and Cities

Tom Ammiano  
City and County of San Francisco

Ralph J. Appezatto  
Cities of Alameda County

Keith Axtell  
U.S. Department of Housing and  
Urban Development

James T. Beall Jr.  
Santa Clara County

Mark DeSaulnier  
Contra Costa County

Bill Dodd  
Napa County and Cities

Dorene M. Giacomini  
U.S. Department of Transportation

Scott Haggerty  
Alameda County

Randell H. Iwasaki  
State Business, Transportation  
and Housing Agency

Barbara Kaufman  
San Francisco Bay Conservation  
and Development Commission

Sue Lempert  
Cities of San Mateo County

John McLemore  
Cities of Santa Clara County

Michael Nevin  
San Mateo County

Jon Rubin  
San Francisco Mayor's Appointee

James P. Spering  
Solano County and Cities

Pamela Torliatt  
Association of Bay Area Governments

Sharon Wright  
Sonoma County and Cities

## *Management Staff*

*Executive Director*  
Steve Heminger

*Deputy Director, Operations*  
Ann Flemer

*Deputy Director, Policy*  
Therese McMillan

*General Counsel*  
Francis Chin

*Manager of Finance*  
Brian Mayhew

*Manager of Bridge  
and Highway Operations*  
Rod McMillan

For additional information, please contact:

**Peter Lee**, Program Manager, RM-1  
(510) 817-3206  
[plee@mtc.ca.gov](mailto:plee@mtc.ca.gov)

# BAY AREA TOLL AUTHORITY

Regional Measure 1  
Toll Bridge Projects

## Project Monitoring Program

APRIL 2002 PROGRESS REPORT



*Prepared for*  
Metropolitan Transportation  
Commission

Bay Area Toll Authority

Joseph P. Bort MetroCenter  
101 Eighth Street  
Oakland, California 94607  
Tel: 510-464-7700  
TDD/TTY: 510-464-7769  
Fax: 510-464-7848  
E-mail: [info@mtc.ca.gov](mailto:info@mtc.ca.gov)  
Web: [www.mtc.ca.gov](http://www.mtc.ca.gov)



*Prepared by*  
Bechtel Infrastructure Corporation



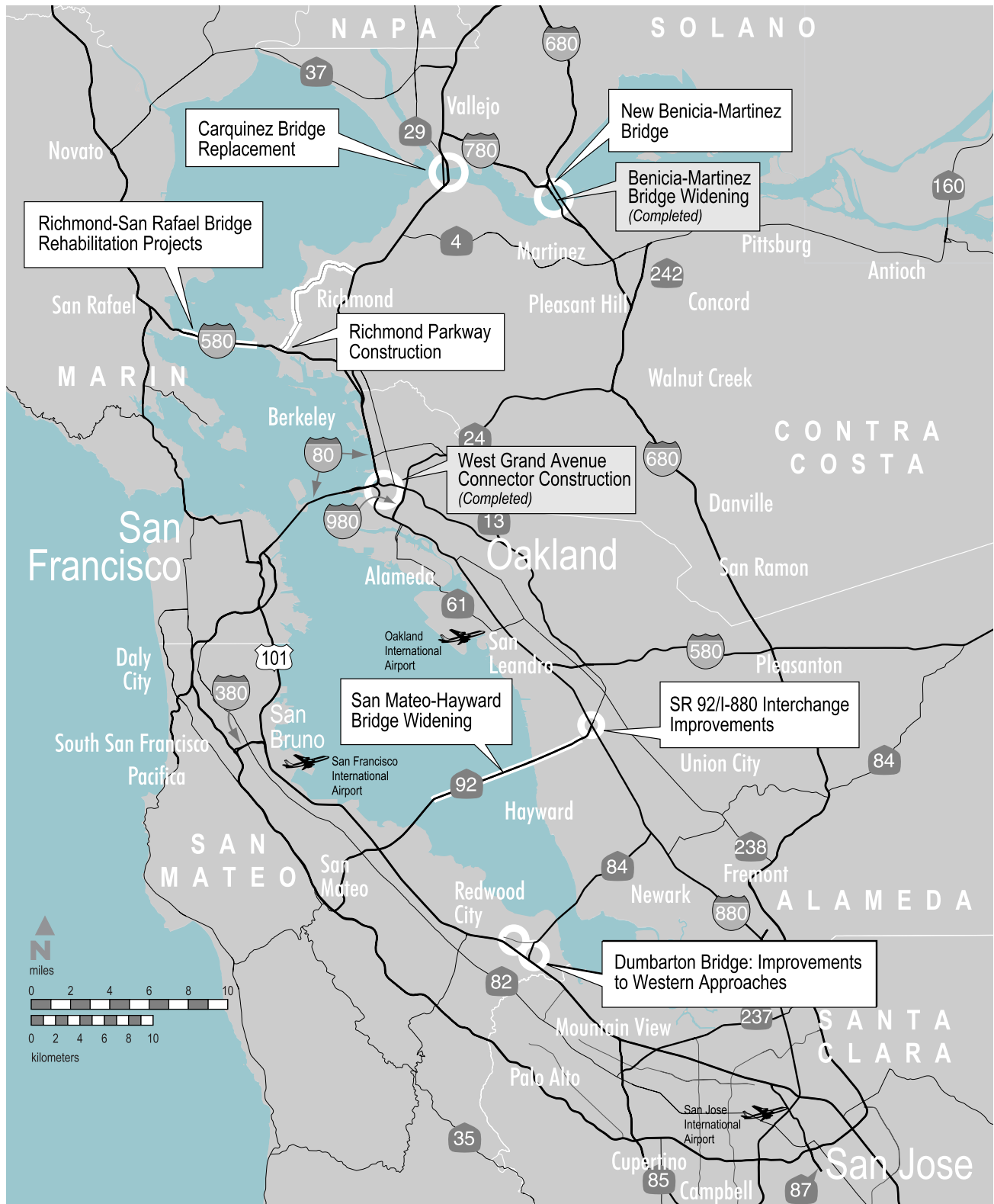
*The following information is provided in accordance with California Government code Section 7550:*

*This document is one of a series of reports prepared for the Bay Area Toll Authority (BATA)/Metropolitan Transportation Commission (MTC) for the Regional Measure 1 Toll Bridge Program. The contract value for the monitoring efforts, technical analysis, and field site work which contribute to these reports, as well as the report preparation and production, is \$3,068,993.00.*

**TABLE OF CONTENTS**

Regional Measure 1 Toll Bridge Program .....	<b>1</b>
Executive Summary - Status of Regional Measure 1 Projects, and Key Issues and Developments	
Northern Bridge Group .....	<b>2</b>
Southern Bridge Group .....	<b>4</b>
Cost Status Summary .....	<b>6</b>
Schedule Status Summary .....	<b>7</b>
Northern Bridge Group	
New Benicia-Martinez Bridge .....	<b>8</b>
Carquinez Bridge Replacement .....	<b>10</b>
Richmond-San Rafael Bridge Rehabilitation .....	<b>12</b>
Southern Bridge Group	
San Mateo-Hayward Bridge Widening .....	<b>14</b>
I-880/SR-92 Interchange Improvement .....	<b>16</b>
Dumbarton Bridge West Approach Projects .....	<b>18</b>
Appendices	
Appendix A: Project Budget Adjustments. ....	<b>A-1</b>
Appendix B: Current Approved Contract Change Orders (CCOs) .....	<b>B-1</b>
Appendix C: Project Cost Summary Details .....	<b>C-1</b>

REGIONAL MEASURE 1 TOLL BRIDGE PROJECTS



## BAY AREA TOLL AUTHORITY

### REGIONAL MEASURE 1 TOLL BRIDGE PROGRAM

In November 1988, Bay Area voters approved Regional Measure 1 (RM 1) and authorized a standard auto toll of \$1 for all seven state-owned Bay Area toll bridges. The additional revenues generated by the toll increase were identified for use for certain highway and bridge improvements, public transit rail extensions, and other projects that reduce congestion on the bridges.

RM 1 identified nine specific bridge and highway improvement projects as eligible for funding from the toll increase. The projects identified by RM 1 and their status of development are depicted in the map on the facing page and can be itemized as follows:

#### ***Northern Bridge Group Projects:***

1. Widening of the existing Benicia-Martinez Bridge (completed; not included in this report)
2. Construction of a new bridge parallel to the existing Benicia-Martinez Bridge
3. Replacement of the existing western span of the Carquinez Bridge
4. Major rehabilitation of the existing Richmond-San Rafael Bridge
  - Trestle and fender rehabilitation
  - Deck rehabilitation
5. Construction of an eastern approach (Richmond Parkway) between the Richmond-San Rafael Bridge and Interstate 80 near Pinole (non-Caltrans project)

#### ***Southern Bridge Group Projects:***

1. Construction of the West Grand Avenue connector to the San Francisco-Oakland Bay Bridge (completed; not included in this report)
2. Widening of the existing San Mateo-Hayward Bridge and eastern approach to six lanes
3. Improvement of the Interstate 880/State Route 92 interchange
4. Improvements to the western approaches to the Dumbarton Bridge
  - US 101/University Avenue interchange reconstruction (non-Caltrans project; completed)
  - State Route 84 (Bayfront Expressway) widening.

### **MTC, BATA, AND THE CALIFORNIA STATE DEPARTMENT OF TRANSPORTATION (CALTRANS)**

As the regional transportation planning agency for the San Francisco Bay Area, MTC is responsible for transportation planning, coordinating, and fund programming in the nine counties surrounding the Bay.

With SB 226, MTC, as the Bay Area Toll Authority (BATA), has also assumed certain duties previously held by the California Transportation Commission, including responsibilities for the programming, administration, and allocation of all toll revenues (excluding the seismic surcharge) from the state-owned toll bridges in the Bay Area.

Caltrans continues to be responsible for the ongoing operation of the state-owned bridges, including the collection of tolls, as well as the maintenance, rehabilitation, and capital improvement of the bridges.







### **PROJECT MONITORING PROGRAM**




This report focuses on monitoring project cost and schedule performance for the Regional Measure 1 Toll Bridge Improvement Projects, as measured against approved budget and schedule milestones. The report provides comprehensive project status information at three levels of detail:

- Color-coded executive summary of Regional Measure 1 projects status (program level)
- Detailed status of individual bridge projects (project level)
- Supplemental project and contract information, including budget adjustments, approved construction change orders, and project and contract level cost data, in the appendices.



**EXECUTIVE SUMMARY – STATUS OF REGIONAL MEASURE 1 PROJECTS**

NORTHERN BRIDGE GROUP	COST	SCHEDULE
New Benicia-Martinez Bridge		
Carquinez Bridge Replacement		
Richmond-San Rafael Bridge Rehabilitation		

- Legend:**
-  Green = no variance to current budget/schedule
  -  Yellow = variance to current budget/schedule (trend), defined as follows:  
For Cost: Project (BATA) contingency use may be required  
For Schedule: Construction contract completion dates delayed by greater than 3 months
  -  Red = variance to current budget/schedule, defined as follows:  
For Cost: Project budget change may be required  
For Schedule: “New facility open to traffic” date delayed by greater than 3 months by non-weather-related issues












BAY AREA TOLL AUTHORITY

EXECUTIVE SUMMARY – STATUS OF KEY ISSUES AND DEVELOPMENTS

- On the main span contract, the drilled piles have been installed at the south abutment and concrete pre-operations have started. Drilled piles and rebar cages are being installed at Pier 5. Driving of steel pipe piles has been temporarily delayed due to environmental issues at Pier 9. Extension of the north temporary trestle continues with installation of finger extensions to construct bridge Piers 16 and 17. Concrete has been placed in the first precast pier footing on Mare Island.
  - On the I-680/I-780 interchange contract, construction commenced on April 1, 2002. Clearing and grubbing, drainage system installation and retaining wall construction are in progress. Mass earthwork near the existing toll plaza is forecast to commence at the end of April.
  - The toll plaza contract was awarded on April 5, 2002. Work is scheduled to commence in early May.
  - The I-680/Marina Vista interchange contract is scheduled to be advertised on April 29, 2002, with a bid opening scheduled for June 26, 2002.
- 
- On the replacement bridge and north approach contract, concrete encasement of the south anchor frames continues. Installation of the upper cross-strut of the north and south towers continues. Stairs are being installed in the tower interiors. Concrete operations are in progress at the architectural wall on the north approach and pile installation is underway at Bent 7 on the south side.
  - On the south approach and interchange contract, the Crockett interchange off-ramp to I-80 and the westbound on-ramp have been demolished. Falsework is being erected for the new Crockett eastbound/westbound on-off ramps.
  - On the maintenance facility contract, punchlist work is in progress on the main and vehicle buildings. Maintenance personnel anticipate occupying the new buildings by the end of April, allowing for the demolition of the old buildings to commence.
- 
- Caltrans is reporting a scheduled completion of mid 2005 for the retrofit contract. The trestle replacement portion of the project is scheduled to be completed in September 2004.
  - Steel encasement of the pier piles is in progress. Spot blasting, cleaning, and concrete coring is underway on the substructure. Installation of stringer restrainers and structural steel is underway on the superstructure. Median barrier demolition of the west bridge trestle is in progress.
  - A “yellow” schedule status code is noted for the project as the schedule for the deck rehabilitation contract is under review based on the later forecasted completion date of the retrofit contract.
  - Noise complaints due to night-time pile driving operations are being addressed by Caltrans. Caltrans is evaluating ways to mitigate the noise issues.

**EXECUTIVE SUMMARY – STATUS OF REGIONAL MEASURE 1 PROJECTS**

SOUTHERN BRIDGE GROUP	COST	SCHEDULE
San Mateo-Hayward Bridge Widening		
I-880/SR-92 Interchange Improvement		
Dumbarton Bridge West Approach Projects (Bayfront Expressway)		

- Legend:**
-  Green = no variance to current budget/schedule
  -  Yellow = variance to current budget/schedule (trend), defined as follows:  
For Cost: Project (BATA) contingency use may be required  
For Schedule: Construction contract completion dates delayed by greater than 3 months
  -  Red = variance to current budget/schedule, defined as follows:  
For Cost: Project budget change may be required  
For Schedule: “New facility open to traffic” date delayed by greater than 3 months by non-weather-related issues

**BAY AREA TOLL AUTHORITY**

**EXECUTIVE SUMMARY – STATUS OF KEY ISSUES AND DEVELOPMENTS**

- On the bridge widening contract, concurrent pile driving, bent cap construction, bridge deck concreting, and cast-in-place concrete barrier rail installation continue. 17,812 feet of paved bridge deck (of 24,837 feet total) is in place.
  - On the mini-toll plaza contract, concrete operations for the toll booths are complete; electrical installation and interior finish work are in progress. Space frame roofing installation is delayed due to a galvanizing problem with the roof fasteners. Pile driving for the overhead walkway is in progress.
  - To facilitate construction of the new mini-toll plaza, Caltrans has temporarily closed toll booth #8. Caltrans is evaluating the impact of the toll booth closure on traffic. Based on the evaluation, Caltrans will decide if the toll booth will remain closed until completion of the construction work in November 2002.
  - On the pedestrian overcrossing contract, piles are installed; earthwork and concrete operations are in progress for the north and south abutments. The center pile for the structure was driven in the median on April 10, 2002.
- 
- Longer than anticipated review of the Supplement to the Draft Environmental Impact Statement/Report (SDEIS/R) by FHWA is delaying issuance of the report. Based on receipt of final FHWA comments, Caltrans plans to issue the SDEIS/R in mid-May 2002.
  - Caltrans continues to perform preliminary engineering in an effort to minimize delays to the project schedule and is approximately 40% complete with plans. However, Caltrans is forecasting a delay of 18 months to the completion date of the project due to the delayed environmental review and anticipated lengthy right-of-way acquisition process. As a result, a "red" schedule status code is indicated in the PMP report.
  - Based on a preliminary revised cost estimate for the project, Caltrans is reporting that the project cost will exceed the current contract budget and will require the use of project contingency. As a result, a "yellow" cost status code is indicated in the PMP report.
  - Caltrans is in the process of procuring consultant services to review the current project plans, schedule and estimates for constructibility and reasonableness. Upon completion by late June, Caltrans and BATA staffs will develop a revised budget and schedule for the project, and develop proposals to potentially expedite the project for review by the Authority.
- 
- The project was advertised on January 14, 2002 and bids were opened on April 3, 2002. The apparent low bid was 4% less than the engineer's estimate. With A+B bidding, construction will be complete by November 2003, 5 months earlier than scheduled. Award of the contract is forecast for May 2, 2002.
  - Additional support budget is necessary to complete the project. Caltrans has indicated the need to utilize the project contingency to cover the increase.

## COST STATUS SUMMARY (MILLION DOLLARS)

PROJECT	Baseline Budget (June 2000)	Current Budget (Apr 2002)	Current Forecast (Apr 2002)	Expended To Date (7/98 - 02/02)
<i>Northern Bridge Group</i>				
New Benicia-Martinez Bridge	586.0	652.8	652.8	89.7
Carquinez Bridge Replacement	433.2	479.8	479.8	247.5
Richmond-San Rafael Bridge Rehabilitation				
▶ West Trestle and Fender Rehabilitation	45.4	35.4	35.4	0.7
▶ Deck Rehabilitation	53.4	53.4	53.4	0.0
Richmond Parkway (Non-Caltrans) <sup>1</sup>	5.9	5.9	5.9	2.4
SUBTOTAL – NBG	<b>1,123.9</b>	<b>1,227.2</b>	<b>1,227.2</b>	<b>340.3</b>
<i>Southern Bridge Group</i>				
San Mateo-Hayward Bridge Widening				
▶ Widening	203.6	217.5	217.5	143.1
▶ West Approach Replacement Planting	0.3	0.3	0.3	0.0
I-880/SR-92 Interchange Improvement	124.2	134.2	134.2	7.0
Dumbarton Bridge West Approach Projects				
▶ US-101/University Avenue Interchange Reconstruction ( <i>Non-Caltrans</i> )	3.8	3.8	3.8	3.7
▶ Bayfront Expressway (SR-84) Widening	33.8	33.8	33.8	4.1
SUBTOTAL – SBG	<b>365.6</b>	<b>389.5</b>	<b>389.5</b>	<b>157.9</b>
<b>GRAND TOTAL</b>	<b>1,489.5</b>	<b>1,616.8</b>	<b>1,616.8</b>	<b>498.2</b>

<sup>1</sup> For the Richmond Parkway, values shown represent the portion funded by BATA.

BAY AREA TOLL AUTHORITY

SCHEDULE STATUS SUMMARY

PROJECT	New Facility Open to Traffic Baseline (June 2000)	New Facility Open to Traffic Current (Apr 2002)	New Facility Open to Traffic Forecast (Apr 2002)
<i>Northern Bridge Group</i>			
New Benicia-Martinez Bridge	Jan 04	Dec 04	Dec 04
Carquinez Bridge Replacement	Jan 03	Oct 03	Oct 03
Richmond-San Rafael Bridge Rehabilitation			
▶ West Trestle and Fender Rehabilitation <sup>1</sup>	Dec 04	Sep 04	Sep 04
▶ Deck Rehabilitation <sup>1</sup>	Sep 06	Sep 06	Jul 07
Richmond Parkway ( <i>Non-Caltrans</i> ) <sup>2</sup>	Feb 01	May 01	May 01
<i>Southern Bridge Group</i>			
San Mateo-Hayward Bridge Widening			
▶ Widening	Dec 02	Dec 02	Dec 02
▶ West Approach Replacement Planting <sup>1</sup>	May 08	May 08	May 08
I-880/SR-92 Interchange Improvement	Dec 06	Dec 06	Jul 08
Dumbarton Bridge West Approach Projects			
▶ US-101/University Avenue Interchange Reconstruction ( <i>Non-Caltrans</i> ) <sup>1</sup>	Apr 00	Apr 00	Apr 00
▶ Bayfront Expressway (SR-84) Widening <sup>1</sup>	Mar 03	Apr 04	Nov 03

<sup>1</sup> For the Richmond-San Rafael Bridge Rehab, San Mateo-Hayward Bridge West Approach Planting, and Dumbarton Bridge West Approach projects, the dates shown reflect construction completion; the existing facility remains open to traffic during all phases of construction.

<sup>2</sup> The Canal Boulevard realignment was opened to traffic on May 25, 2001.

## NEW BENICIA-MARTINEZ BRIDGE

The existing Benicia-Martinez Bridge provides Interstate 680 access across the Carquinez Strait between Contra Costa and Solano counties. This project will construct a new parallel Benicia-Martinez Bridge east of the existing span that will include the following features:

- Five northbound lanes on the new bridge (four mixed-flow lanes and one slow-vehicle lane) that will allow conversion of the existing bridge to southbound traffic only
- A new bicycle/pedestrian lane on the existing bridge
- A new toll plaza south of the new bridge in Contra Costa County with 17 toll booths, including two high-occupancy-vehicle (HOV) bypass lanes
- Reconstruction of the Interstate 680/Marina Vista Road and Interstate 680/Interstate 780 interchanges



*North Temporary Trestle with Finger Extensions*

### Project Photos



*Excavation of Steel Piles*



*Precast Pier Footing Construction on Mare Island*

### Current Activities:

- On the main span contract, the cast-in-drilled-hole piles have been installed at the south abutment and excavation has started for the footing concrete operations. Cast-in-drilled-hole piles and rebar cages are being installed at Pier 5. Driving of steel pipe piles at Pier 9 has been temporarily halted due to environmental issues. Extension of the north temporary trestle continues and finger extensions are being installed to construct bridge Piers 16 and 17. Pile testing is in progress, with results expected by the end of April. Concrete has been placed in the first precast pier footing on Mare Island.
- On the I-680/I-780 interchange contract, physical construction commenced on April 1, 2002 with re-striping, K-rail placement, and demolition of the center barrier on I-780, clearing and grubbing on I-680 and on I-780, and drainage system and retaining wall installation on I-680. Mass earthwork near the existing toll plaza is forecast to commence at the end of April.
- The toll plaza contract was awarded on April 5, 2002. Work is scheduled to commence in early May.
- The I-680/Marina Vista interchange contract is scheduled to be advertised on April 29, 2000, with a bid opening scheduled for June 26, 2002.

BAY AREA TOLL AUTHORITY

NEW BENICIA-MARTINEZ BRIDGE

COST STATUS (\$ Million)	Baseline Budget (June 2000)	Current Budget (Apr 2002)	Current Forecast (Apr 2002)	Variance	Expended to Date (7/98 - 02/02)	Notes
Capital Outlay Construction						
South Approach	6.0	7.0	7.0	0.0	6.3	
New Bridge	247.3	306.7	306.7	0.0	27.5	
Toll Plaza and Administration Building	22.8	29.7	21.2	-8.5	0.0	A
I-680/Marina Vista Interchange	43.2	51.3	51.3	0.0	0.0	
I-680/I-780 Interchange	80.8	54.7	54.7	0.0	0.1	
Other Budgeted Capital	28.1	30.0	30.0	0.0	1.7	
Capital Outlay Support	78.2	99.1	99.1	0.0	43.5	
Capital ROW	21.1	14.4	14.4	0.0	10.6	
Non-BATA Funding	0.0	31.0	31.0	0.0		
Project (BATA) Contingency	58.4	28.8	37.3	8.5		
<b>Project Total (a)</b>	<b>586.0</b>	<b>652.8</b>	<b>652.8</b>	<b>0.0</b>	<b>89.7</b>	
(a) Totals may be rounded						

SCHEDULE STATUS	Baseline (June 2000)	Current (Apr 2002)	Forecast (Apr 2002)	Variance	Notes
<b>Construction Contract Completion</b>					
New Bridge	Jan 04	Sep 04	Sep 04	0	
Toll Plaza and Administration Building	Feb 03	Sep 04	Sep 04	0	
I-680/Marina Vista Interchange	Dec 03	Nov 04	Dec 04	+1	
I-680/I-780 Interchange	Dec 03	Dec 04	Dec 04	0	
South Approach	Mar 01	Oct 01	Oct 01	0	
Modify Existing Bridge	Jul 05	Dec 06	Dec 06	0	
<b>Project</b>					
<b>New Facility Open to Traffic</b>	<b>Jan 04</b>	<b>Dec 04</b>	<b>Dec 04</b>	<b>0</b>	

NOTES

ACTION

A. The low bidder was approximately \$8.5 million less than the engineer's estimate. BATA will adjust the contract budget after receipt of the contract award summary.

None at this time.



## CARQUINEZ BRIDGE REPLACEMENT

The existing Carquinez Bridge carries Interstate 80 traffic between the cities of Vallejo and Crockett on two separate bridges: the 1927 bridge for westbound traffic and the 1958 bridge for eastbound traffic. While the 1958 bridge is being strengthened under Caltrans' seismic retrofit program, the 1927 bridge has been identified as being seismically deficient and will be replaced under the RM 1 program. The Carquinez Bridge replacement project will construct a new suspension bridge west of the existing bridges. The existing 1927 bridge will be demolished after the completion of this project. The project will incorporate the following features:

- Four westbound lanes on the new bridge (three mixed-flow lanes and one HOV lane)
- A new bicycle/pedestrian lane on the new bridge

### Project Photos



*View South to North*



*South Approach – Falsework*



*North Tower – Upper Cross-Strut*

### Current Activities:

- On the replacement bridge and north approach contract, anchor rods are being installed at the north anchorage and concrete encasement continues at the south anchor frames. Concrete operations continue on the upper strut of the north and south towers with form installation and pouring of walls, slabs, and decks. Concrete operations on both towers are scheduled to be completed within the next two months. Stairs are being installed in the tower interiors. Form and rebar installation, and concrete placement for the architectural wall on the north approach is in progress. At Bent 7, on the south side, cast-in-steel-shell piles are being installed.
- On the south approach and interchange contract, the Crockett interchange off-ramp to I-80 and the westbound on-ramp have been demolished. Falsework erection for concrete operations continues on both Crockett eastbound/westbound on/off ramps. Pile driving at the west abutment footing for the viaduct is complete. The sheet piles and bracing for the excavation retaining structure at Bent 6 are installed; driving of steel pipe piles will continue through April. Barrier removal and excavation in the freeway median between Cummings Skyway and the bridge are scheduled to be completed by the end of April.
- On the maintenance facility contract, punchlist work is in progress on the main and vehicle buildings. Maintenance personnel anticipate occupying the new buildings by the end of April, allowing for demolition of the old buildings to commence. Mechanical/electrical installation is complete at the fuel island; concrete operations are in progress. The perimeter property fence is being installed.

BAY AREA TOLL AUTHORITY

CARQUINEZ BRIDGE REPLACEMENT

COST STATUS (\$ Million)	Baseline Budget (June 2000)	Current Budget (Apr 2002)	Current Forecast (Apr 2002)	Variance	Expended to Date (7/98 - 02/02)	Notes
Capital Outlay Construction						
Replacement Bridge and North Approach	213.7	243.1	243.1	0.0	149.4	
South Approach and Interchange	116.0	73.9	73.9	0.0	21.2	
Maintenance Facility (Phases I & II)	7.0	8.1	8.1	0.0	5.8	
1927 Bridge Demolition	16.0	16.0	16.0	0.0	0.0	
Other Budgeted Capital	10.6	13.6	13.6	0.0	5.8	
Capital Outlay Support	43.7	96.8	96.8	0.0	56.4	
Capital ROW	9.6	11.1	11.1	0.0	8.9	
Project (BATA) Contingency	16.5	17.1	17.1	0.0		
<b>Project Total (a)</b>	<b>433.2</b>	<b>479.8</b>	<b>479.8</b>	<b>0.0</b>	<b>247.5</b>	

(a) Totals may be rounded

SCHEDULE STATUS	Baseline (June 2000)	Current (Apr 2002)	Forecast (Apr 2002)	Variance	Notes
<b>Construction Contract Completion</b>					
Replacement Bridge & North Approach	Dec 03	May 04	May 04	0	
South Approach and Interchange	Oct 04	Jul 03	Jul 03	0	
Maintenance Facility	Mar 02	Jun 02	Jun 02	0	
1927 Bridge Demolition	Mar 06	Mar 06	Mar 06	0	
<b>Project</b>					
New Facility Open to Traffic	Jan 03	Oct 03	Oct 03	0	

NOTES

ACTION

None for this report.

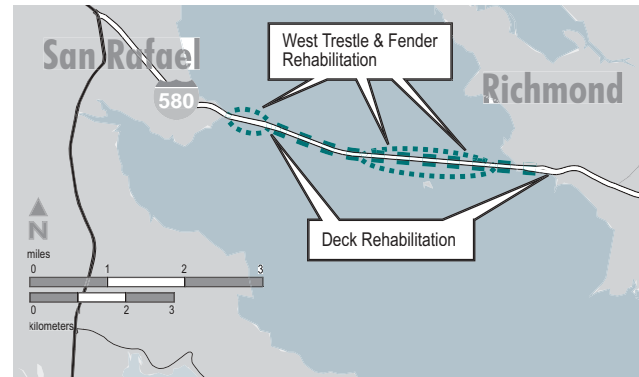
### RICHMOND-SAN RAFAEL BRIDGE REHABILITATION

Completed in 1956, the Richmond-San Rafael Bridge provides access - via Interstate 580 - across San Francisco Bay between Marin and Contra Costa counties. Major rehabilitation of the bridge was specified in RM 1 to be eligible for toll funding.

Currently, there are two major rehabilitation projects planned for the bridge:

- The first project is to rehabilitate the western trestle (low-rise) section of the bridge near San Rafael and the ship collision protection fender system at various bridge piers. This rehabilitation work is being combined with a seismic retrofit project.
- The second project is to rehabilitate the existing concrete deck on the bridge. The cast-in-place concrete riding surface (deck) has been worn down

over time due to traffic and exposure to the marine environment. This project will follow the completion of the trestle and fender rehabilitation project to avoid possible construction conflicts.



### Project Photos



*Underside Work Platforms*



*Micro-pile Installation*

### Current Activities:

- Work is progressing with dredging, installation of micro-piles, and steel retrofit encasement of the pier piles. Spot blasting, cleaning, and concrete coring for the steel bents continues on the substructure. Installation of exterior stringer restrainers and structural steel continue on the superstructure. A temporary work trestle, under construction on the western side of the bridge, is being used as the platform for median barrier demolition of the west bridge trestle.
- Caltrans has received noise complaints due to night-time pile driving operations. Pile driving is being performed at night as partial bridge closures are needed to provide a safe working environment for the driving public and construction workers. Caltrans is evaluating ways to mitigate the noise issues.

BAY AREA TOLL AUTHORITY

RICHMOND-SAN RAFAEL BRIDGE REHABILITATION

COST STATUS (\$ Million)	Baseline Budget (June 2000)	Current Budget (Apr 2002)	Current Forecast (Apr 2002)	Variance	Expended to Date (7/98 - 02/02)	Notes
<b>West Trestle and Fender Rehabilitation</b>						
Capital Outlay Construction	33.9	0.0	0.0	0.0	0.0	
Capital Outlay Support	5.4	0.7	0.7	0.0	0.7	
Capital ROW	0.0	0.0	0.0	0.0	0.0	
Non-BATA Funding	0.0	34.6	34.6	0.0		
Project (BATA) Contingency	6.1	0.0	0.0	0.0		
<b>Subtotal</b>	<b>45.4</b>	<b>35.4</b>	<b>35.4</b>	<b>0.0</b>	<b>0.7</b>	
<b>Deck Rehabilitation</b>						
Capital Outlay Construction	33.0	33.0	33.0	0.0	0.0	
Capital Outlay Support	9.0	5.0	5.0	0.0	0.0	
Capital ROW	0.0	0.0	0.0	0.0	0.0	
Non-BATA Funding	0.0	4.0	4.0	0.0		
Project (BATA) Contingency	11.4	11.4	11.4	0.0		
<b>Subtotal</b>	<b>53.4</b>	<b>53.4</b>	<b>53.4</b>	<b>0.0</b>	<b>0.0</b>	
<b>Project Total (a)</b>	<b>98.9</b>	<b>88.8</b>	<b>88.8</b>	<b>0.0</b>	<b>0.7</b>	

(a) Totals may be rounded

SCHEDULE STATUS	Baseline (June 2000)	Current (Apr 2002)	Forecast (Apr 2002)	Variance	Notes
<b>Construction Contract Completion</b>					
West Trestle and Fender Rehabilitation	Dec 04	Sep 04	Sep 04	0	
Deck Rehabilitation	Sep 06	Sep 06	Jul 07	+10	A
<b>Project</b>					
<b>New Facility Open to Traffic</b>					B

NOTES	ACTION
A. Caltrans is reporting a forecast completion in mid-2005 for the retrofit project with the deck rehabilitation to follow.	Caltrans is reviewing the forecast completion date of the deck rehabilitation contract.
B. The existing facility remains open to traffic during all phases of construction.	None.

## SAN MATEO-HAYWARD BRIDGE WIDENING

In 1996, the western approach to the San Mateo-Hayward Bridge from U.S. Route 101 was widened from four to six lanes to match the configuration of the high-rise section of the bridge. The current project will complete the widening work in this corridor along the low-trestle section of the bridge and its eastern approach to Interstate 880. The project includes the following features:

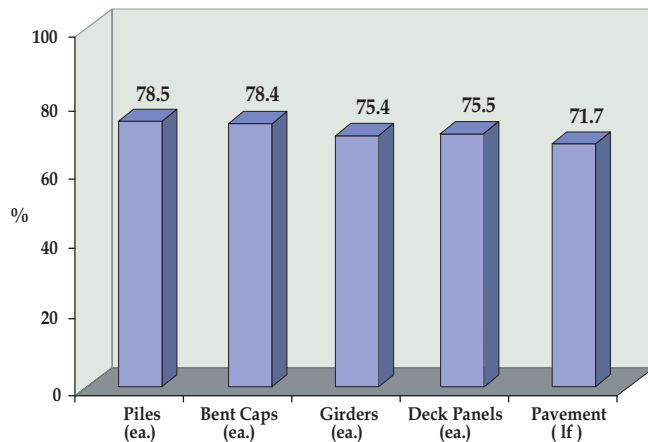
- Widening of the low-rise trestle and eastern approach from I-880 from four to six lanes with shoulders
- Extension of the existing west-bound HOV lane one mile west along the eastern approach from Interstate 880
- Construction of a new pedestrian/bicycle overcrossing of State Route 92
- Expansion of the existing toll plaza

- Improvements to the Hayward Shoreline Interpretive Center
- Also, as an associated project, improvements will be made to the Bay Trail between the San Mateo-Hayward and Dumbarton Bridges in the East Bay.



Mini Toll Plaza – Space Frame Ready for Roof Installation

### Project Construction Progress and Photos



Construction Progress – Percent Complete (as of 4/15/02)



Pedestrian Overcrossing – Falsework for South Abutment Ramp

### Current Activities:

- On the bridge widening contract, concurrent pile driving, bent cap construction, bridge deck concreting and cast-in-place concrete barrier rail installation continue. 17,812 feet of paved bridge deck (of 24,837 feet total) is in place.
- On the mini toll plaza contract, toll booth concrete operations are complete; electrical installation and interior finish work is in progress. The metal roofing installation on the space frame is delayed due to a problem with galvanization of the fasteners. Pre-drilling and pile driving for the overhead walkway will continue into early May.
- To facilitate construction of the new mini-toll plaza, Caltrans has temporarily closed toll booth #8. Caltrans is evaluating closure of the toll booth until completion of construction in November 2002.
- On the pedestrian overcrossing contract, piles are installed; earthwork and concrete operations are in progress for the north and south abutments. Ramp falsework is being erected at the south abutment. The center pile for the structure was driven in the median on April 10, 2002.

BAY AREA TOLL AUTHORITY

SAN MATEO-HAYWARD BRIDGE WIDENING

COST STATUS (\$ Million)	Baseline Budget (June 2000)	Current Budget (Apr 2002)	Current Forecast (Apr 2002)	Variance	Expended to Date (7/98 - 02/02)	Notes
<b>Widening</b>						
Capital Outlay Construction						
Widen Trestle	124.8	128.9	128.9	0.0	92.8	
Widen Roadway	29.2	26.0	26.0	0.0	25.0	
Construct Mini Toll Plaza	4.4	6.3	6.3	0.0	2.2	
Other Budgeted Capital	8.9	8.9	8.9	0.0	0.0	
Capital Outlay Support	15.5	31.6	31.6	0.0	22.5	
Capital ROW	1.5	1.4	1.4	0.0	0.5	
Project (BATA) Contingency	19.3	14.4	14.4	0.0		
<b>Subtotal</b>	<b>203.6</b>	<b>217.5</b>	<b>217.5</b>	<b>0.0</b>	<b>143.1</b>	
<b>West Approach Planting</b>						
Capital Outlay Construction	0.2	0.2	0.2	0.0	0.0	
Capital Outlay Support	0.1	0.1	0.1	0.0	0.0	
Capital ROW	0.0	0.0	0.0	0.0	0.0	
Project (BATA) Contingency	0.1	0.1	0.1	0.0		
<b>Subtotal</b>	<b>0.3</b>	<b>0.3</b>	<b>0.3</b>	<b>0.0</b>	<b>0.0</b>	
<b>Project Total (a)</b>	<b>203.9</b>	<b>217.8</b>	<b>217.8</b>	<b>0.0</b>	<b>143.1</b>	
(a) Totals may be rounded						

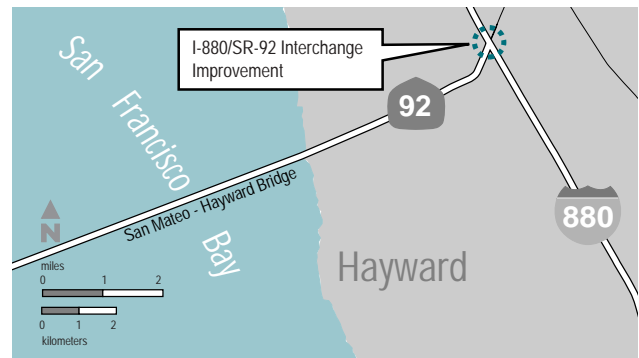
SCHEDULE STATUS	Baseline (June 2000)	Current (Apr 2002)	Forecast (Apr 2002)	Variance	Notes
<b><u>Construction Contract Completion</u></b>					
<b>Widening</b>					
Widen Trestle	Dec 02	May 03	May 03	0	
Construct Mini Toll Plaza	Nov 02	Nov 02	Nov 02	0	
Widen Eastern Approach	Nov 02	Sep 01	Sep 01	0	
Pedestrian Overcrossing	Sep 02	Sep 02	Sep 02	0	
<b>West Approach Planting</b>					
West Approach Replacement Planting	May 08	May 08	May 08	0	
<b><u>Project</u></b>					
New Facility Open to Traffic	Dec 02	Dec 02	Dec 02	0	

NOTES	ACTION
-------	--------



## I-880/SR-92 INTERCHANGE IMPROVEMENT

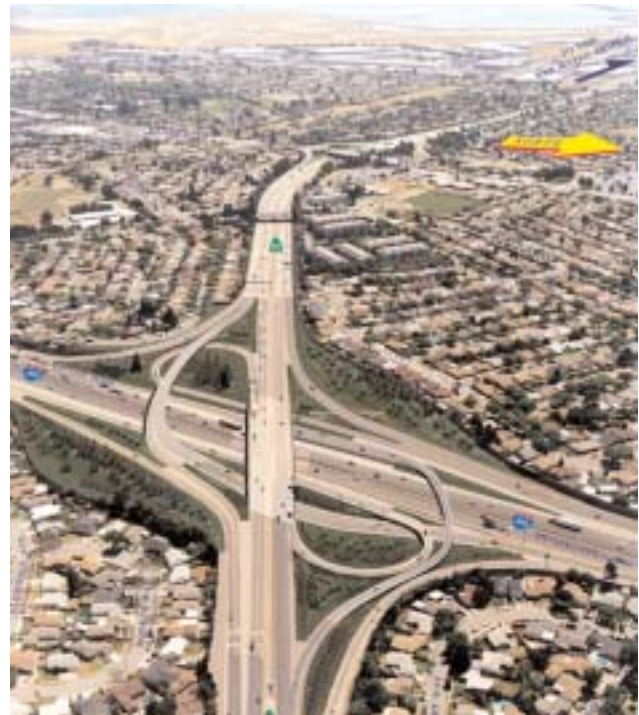
As part of an effort to improve traffic flow and to relieve congestion on the San Mateo-Hayward Bridge and its approaches, RM 1 identified the need to improve and reconstruct the Interstate 880/State Route 92 interchange. While still in the environmental clearance phase, the project proposes to modify the existing cloverleaf interchange to increase capacity and improve safety and traffic operations. Various alternatives are under consideration and are being assessed as part of the environmental process.



### Project Photos



*Existing interchange; looking west*



*Alternative H interchange; looking west*

### Current Activities:

- Longer than anticipated FHWA review of the Supplement to the Draft Environmental Impact Statement Report (SDEIS/R) is delaying issuance of the report. Caltrans will release the SDEIS/R in mid-May 2002, based on final review and approval by the FHWA.
- Caltrans is currently reviewing the project schedule. Due to environmental delays and extensive right-of-way activities, the project completion date is forecast to be delayed 18 months, even with preliminary engineering continuing. The latest forecast starts construction in August 2004 with a construction completion in July 2008.
- Caltrans is in the process of procuring consultant services to review the current project plans, schedule and estimates for constructibility and reasonableness. Upon completion by late June, BATA will be requested to adopt a new budget and schedule.



BAY AREA TOLL AUTHORITY

I-880/SR 92 INTERCHANGE IMPROVEMENT

COST STATUS (\$ Million)	Baseline Budget (June 2000)	Current Budget (Apr 2002)	Current Forecast (Apr 2002)	Variance	Expended to Date (7/98 - 02/02)	Notes
<b>I-880/SR-92 Interchange Improvement</b>						
Capital Outlay Construction	70.3	70.3	94.4	24.1	0.0	A
Capital Outlay Support	20.8	20.8	24.1	3.3	7.0	A
Capital ROW	8.0	8.0	0.0	-8.0	0.0	A
Non-BATA Funding	0.0	10.0	10.0	0.0	0.0	
Project (BATA) Contingency	25.1	25.1	5.7	-19.4	0.0	
<b>Project Total (a)</b>	<b>124.2</b>	<b>134.2</b>	<b>134.2</b>	<b>0.0</b>	<b>7.0</b>	

(a) Totals may be rounded

SCHEDULE STATUS	Baseline (June 2000)	Current (Apr 2002)	Forecast (Apr 2002)	Variance	Notes
<b><u>Construction Contract Completion</u></b>					
I-880/SR-92 Interchange Improvement	Dec 06	Dec 06	Jul 08	+18	B
<b><u>Project</u></b>					
New Facility Open to Traffic	Dec 06	Dec 06	Jul 08	+18	B

NOTES	ACTION
A. A preliminary revised Caltrans estimate for the project is higher than the current budget.	Caltrans is procuring consultant services to review the estimate and schedule.
B. Caltrans is currently updating the project schedule to reflect the current forecast.	As above.

## DUMBARTON BRIDGE WEST APPROACH PROJECTS

RM 1 identified the need for improvements to the western approaches to the Dumbarton Bridge. Through coordination with the City/County Association of Governments of San Mateo County, two projects were identified for toll bridge funding:

- The first project, sponsored by the City of East Palo Alto, modifies the U.S. Route 101/University Avenue interchange. This project is designed to help alleviate traffic congestion and improve overall traffic flow to University Avenue and the Dumbarton Bridge.
- The second project, sponsored by Caltrans, widens the Bayfront Expressway (State Route 84) from the Dumbarton Bridge to the U.S. 101/Marsh Road interchange. The existing six-lane expressway section from the bridge to University Avenue will be reconstructed with upgraded shoulders and lane widths. The existing expressway section from University Avenue to Marsh Road will be widened from four to six lanes.



## Project Photos



Existing SR-84 (Bayfront) at Chrysler



Existing SR-84 (Bayfront) looking west

## Current Activities:

- The Bayfront Expressway widening contract was advertised on January 14, 2002, with bids opened on April 3, 2002. The apparent low bid was 4% less than the engineer's estimate. With A+B bidding, construction will be complete by November 2003, 5 months early. Award of the contract is forecast for May 2, 2002.

BAY AREA TOLL AUTHORITY

DUMBARTON BRIDGE WEST APPROACH PROJECTS

COST STATUS (\$ Million)	Baseline Budget (June 2000)	Current Budget (Apr 2002)	Current Forecast (Apr 2002)	Variance	Expended to Date (7/98 - 02/02)	Notes
<b>US101/University Avenue Interchange Reconstruction (Non-Caltrans)</b>						
Capital Outlay Construction	3.8	3.8	3.8	0.0	3.7	
Capital Outlay Support	0.0	0.0	0.0	0.0	0.0	
Capital ROW	0.0	0.0	0.0	0.0	0.0	
Project (BATA) Contingency	0.0	0.0	0.0	0.0		
<b>Subtotal</b>	<b>3.8</b>	<b>3.8</b>	<b>3.8</b>	<b>0.0</b>	<b>3.7</b>	
<b>Bayfront Expressway (SR84) Widening</b>						
Capital Outlay Construction	24.8	26.7	25.3	-1.4	0.0	
Capital Outlay Support	4.4	4.7	5.7	1.0	4.0	A
Capital ROW	1.3	0.1	0.1	0.0	0.0	
Project (BATA) Contingency	3.3	2.4	2.7	+0.3		
<b>Subtotal</b>	<b>33.8</b>	<b>33.8</b>	<b>33.8</b>	<b>0.0</b>	<b>4.1</b>	
<b>Project Total (a)</b>	<b>37.6</b>	<b>37.6</b>	<b>37.6</b>	<b>0.0</b>	<b>7.8</b>	
(a) Totals may be rounded						

SCHEDULE STATUS	Baseline (June 2000)	Current (Apr 2002)	Forecast (Apr 2002)	Variance	Notes
<b><u>Construction Contract Completion</u></b>					
US 101/University Avenue Interchange Reconstruction (Non-Caltrans)	Apr 00	Apr 00	Apr 00	0	
Bayfront Expressway (SR84) Widening	Mar 03	Apr 04	Nov 03	-5	
<b><u>Project</u></b>					
New Facility Open to Traffic	Mar 03	Apr 04	Nov 03	-5	

NOTES	ACTION
-------	--------

A. Caltrans has reported that additional support funding is necessary. The additional cost will be covered by construction savings and project contingency.

BATA is reviewing the Caltrans forecast.

## APPENDICES

Appendix A: Project Budget Adjustments

Appendix B: Current Approved Contract Change Orders

Appendix C: Project Cost Summary Details



**BAY AREA TOLL AUTHORITY**

**APPENDIX A – LIST OF PROJECT BUDGET ADJUSTMENTS**

**DEFINITIONS:**

**Baseline Budget (June 2000):** Baseline budgets as established by BATA in June 2000 for each project within the overall program.

**Current Budget:** Budget currently serving as the baseline for monitoring purposes. The current budget is equal to the established June 2000 budget, plus or minus any adjustment as listed below.

**Project Baseline Adjustments**

Date	Description of Adjustment
10/2000	Current Budgets for allocated capital outlay adjusted by BATA to reflect current Caltrans construction contract allotments.
11/2000	Current Budgets for the Benicia-Martinez and Richmond-San Rafael Bridge projects revised to reflect new funding from Non-BATA sources per agreements with Caltrans and a Prop. 192 refund from Caltrans related to the Carquinez Bridge project.
01/2001	Current Budget for the Benicia-Martinez bridge revised to reflect allocations by BATA.
03/2001	Current Budget for the Carquinez Bridge revised by BATA.
10/2001	Current Budget for the Benicia-Martinez Bridge revised by BATA.
12/2001	Current Budgets for the Benicia-Martinez and San Mateo-Hayward Bridges revised by BATA.

**BAY AREA TOLL AUTHORITY**

**APPENDIX B – APPROVED CONTRACT CHANGE ORDERS (CCO)  
FOR APRIL 2002  
BENICIA-MARTINEZ BRIDGE**

Bridge/ Contract	CCO Number <sup>1</sup>	CCO Description	Funding Source <sup>2</sup>	Impacts	
				Cost (\$1000's)	Schedule
<b>Main Span 04-006034</b>	16	Modify Special Provision 10-1.43, Grout Testing	-	0.0	
	20	Modify Special Provision 10-1.56, Epoxy Coating on Rebar	-	0.0	
<b>Subtotal</b>				<b>0.0</b>	
<b>Toll Plaza 04-006044</b>		No approved contract change orders for April 2002.			
<b>Subtotal</b>					
<b>I-680/I-780 Interchange 04-006064</b>	1	Flagging and Traffic Control	S	50.0	
	2	Federal Apprentice Program	C	18.4	
	3	Partnering	S	20.0	
	6	Utilities & Non Highway Facilities	S	50.0	
	7	Buried Man-Made Objects	S	20.0	
<b>Subtotal</b>				<b>158.4</b>	
<b>South Approach 04-006094</b>		Contract is complete.			
<b>Totals for April 2002<sup>3</sup></b>				<b>158.4</b>	

<sup>1</sup> Only approved CCOs for the month are listed in Appendix B.

<sup>2</sup> Funding Source(s)  
S = Supplemental work  
C = Construction contingency

<sup>3</sup> Approved CCO cost total is within the approved current contract budget.

**BAY AREA TOLL AUTHORITY**

**APPENDIX B – APPROVED CONTRACT CHANGE ORDERS (CCO)  
FOR APRIL 2002  
CARQUINEZ BRIDGE**

Bridge/ Contract	CCO Number <sup>1</sup>	CCO Description	Funding Source <sup>2</sup>	Impacts	
				Cost (\$1000's)	Schedule
<b>Carquinez Bridge Replacement 04-013014</b>		No approved contract change orders for April 2002			
<b>Subtotal</b>					
<b>South Approach and Crockett Interchange 04-013054</b>	49	Salvage WB Off-Ramp Railing	C	25.0	
	52-S1	TIA #5 Extra Thick Asphalt	C	27.6	
	89	WB Off-Ramp Existing Pile Removal	C	50.0	
<b>Subtotal</b>				<b>102.6</b>	
<b>Maintenance Facility 04-013084</b>		No approved contract change orders for April 2002.			
<b>Subtotal</b>					
<b>Totals for April 2002<sup>3</sup></b>				<b>102.6</b>	

<sup>1</sup> Only approved CCOs for the month are listed in Appendix B.

<sup>2</sup> Funding Source(s)  
S = Supplemental work  
C = Construction contingency

<sup>3</sup> Approved CCO cost total is within the approved current contract budget.



**BAY AREA TOLL AUTHORITY**

**APPENDIX B – APPROVED CONTRACT CHANGE ORDERS (CCO)  
FOR APRIL 2002  
SAN MATEO-HAYWARD BRIDGE**

Bridge/ Contract	CCO Number <sup>1</sup>	CCO Description	Funding Source <sup>2</sup>	Impacts	
				Cost (\$1000's)	Schedule
<b>Widen Trestle 04-045014</b>	49	Enlarge Bridge Deck Concrete Pour-Backs	C	23.4	
<b>Subtotal</b>				<b>23.4</b>	
<b>Mini-Toll Plaza 04-045024</b>		No approved contract change orders for April 2002			
<b>Subtotal</b>					
<b>Pedestrian Overcrossing 04-045044</b>		No approved contract change orders for April 2002			
<b>Subtotal</b>					
<b>Widen Roadway 04-045034</b>		Contract is complete.			
<b>Totals for April 2002<sup>3</sup></b>				<b>23.4</b>	

<sup>1</sup> Only approved CCOs for the month are listed in Appendix B.

<sup>2</sup> Funding Source(s)  
S = Supplemental work  
C = Construction contingency

<sup>3</sup> Approved CCO cost total is within the approved current contract budget.

## BAY AREA TOLL AUTHORITY

## APPENDIX C – PROJECT COST SUMMARY DETAILS

The following sheets provide detailed cost information for each of the bridge projects. Capital Outlay Support (Engineering), Capital Right-of-Way and Capital Outlay are shown for each of the projects. The Project (BATA) Contingency, as well as the bridge projects totals, is shown at the end of each bridge summary.

**Definitions:**

1. Capital Outlay Support includes both Caltrans and Architectural/Engineering (A/E) consultant support.
2. Capital Right of Way includes acquisition, loss of goodwill, demolition, utilities relocations, relocation assistance, title costs, and sale of excess lands.
3. Capital Outlay includes construction bid terms, external third party agreements, state-furnished materials, supplemental construction work, and construction contingencies.

Project Cost Summaries (\$ Millions)						
EA Number	Baseline Budget Jun-00	Current Budget Apr-02	Current Forecast Apr-02	Net Change	Expended <sup>1</sup> to Date (7/98 – 02/02)	Note
<b>Northern Bridge Group</b>						
<i>Project 2003 – New Benicia-Martinez Bridge</i>						
<b>South Approach</b>						
Capital Outlay Support	00609x	3.5	4.0	4.0	0.0	3.5
Capital Right of Way	006099	7.0	7.7	7.7	0.0	7.8
Capital Outlay	006094	6.0	7.0	7.0	0.0	6.3
<b>Total South Approach</b>		<b>16.5</b>	<b>18.8</b>	<b>18.8</b>	<b>0.0</b>	<b>17.7</b>
<b>New Bridge</b>						
Capital Outlay Support	00603x	31.9	36.0	36.0	0.0	14.6
Capital Right of Way	006039	4.1	2.5	2.5	0.0	0.7
Capital Outlay	006034	247.3	306.7	306.7	0.0	27.5
Non-BATA Funding		0.0	10.1	10.1	0.0	
<b>Total New Bridge</b>		<b>283.3</b>	<b>355.3</b>	<b>355.3</b>	<b>0.0</b>	<b>42.8</b>
<b>Toll Plaza &amp; Administration Building</b>						
Capital Outlay Support	00604x	6.2	10.3	10.3	0.0	4.9
Capital Right of Way	006049	0.0	0.0	0.0	0.0	0.0
Capital Outlay	006044	22.8	29.7	21.2	-8.5	0.0
<b>Total Toll Plaza &amp; Admin.</b>		<b>29.1</b>	<b>40.0</b>	<b>31.5</b>	<b>-8.5</b>	<b>4.9</b>
<b>I-680/Marina Vista Interchange</b>						
Capital Outlay Support	00605x	10.7	15.7	15.7	0.0	7.8
Capital Right of Way	006059	7.4	2.5	2.5	0.0	0.8
Capital Outlay	006054	43.2	51.3	51.3	0.0	0.0
<b>Total I-680/MV I/C</b>		<b>61.4</b>	<b>69.5</b>	<b>69.5</b>	<b>0.0</b>	<b>8.7</b>
<b>I-680/I-780 Interchange</b>						
Capital Outlay Support	00606x	18.7	25.7	25.7	0.0	11.6
Capital Right of Way	006069	1.7	1.7	1.7	0.0	1.2
Capital Outlay	006064	80.8	54.7	54.7	0.0	0.1
Non-BATA Funding		0.0	20.9	20.9	0.0	
<b>Total I-680/I-780 I/C</b>		<b>101.2</b>	<b>103.0</b>	<b>103.0</b>	<b>0.0</b>	<b>12.9</b>

<sup>1</sup> Unaudited

BAY AREA TOLL AUTHORITY

Appendix C – Project Cost Summary Details (cont'd)

	EA Number	Baseline Budget Jun-00	Current Budget Apr-02	Current Forecast Apr-02	Net Change	Expended to Date (7/98 – 02/02)	Note
<b>Project 2003 – New Benicia-Martinez Bridge (cont'd)</b>							
<b>Other Budgeted Capital</b>							(a)
Capital Outlay Support		7.1	7.4	7.4	0.0	1.1	
Capital Right of Way		0.9	0.0	0.0	0.0	0.0	
Capital Outlay		28.1	30.0	30.0	0.0	1.7	
<b>Total Other Budgeted Capital</b>		<b>36.1</b>	<b>37.4</b>	<b>37.4</b>	<b>0.0</b>	<b>2.8</b>	
Total Capital Outlay Support		78.2	99.1	99.1	0.0	43.5	
Total Capital Right of Way		21.1	14.4	14.4	0.0	10.6	
Total Capital Outlay		428.2	479.5	471.0	-8.5	35.6	
Non-BATA Funding		0.0	31.0	31.0	0.0	0.0	
Project Contingency (BATA)		58.4	28.8	37.3	8.5		
<b>Total New Benicia-Martinez Bridge</b>		<b>586.0</b>	<b>652.8</b>	<b>652.8</b>	<b>0.0</b>	<b>89.7</b>	
<b>Project 3002 – Carquinez Bridge Replacement</b>							
<b>Replacement Bridge &amp; North Approach</b>							(b)
Capital Outlay Support	01301x	17.7	47.7	47.7	0.0	34.4	
Capital Right of Way	013019	3.0	3.0	3.0	0.0	3.0	
Capital Outlay	013014	213.7	243.1	243.1	0.0	149.4	
<b>Total Replacement Bridge &amp; N. Approach</b>		<b>234.4</b>	<b>293.9</b>	<b>293.9</b>	<b>0.0</b>	<b>186.8</b>	
<b>South Approach &amp; Interchange</b>							(c)
Capital Outlay Support	01305x	22.7	32.4	32.4	0.0	17.0	
Capital Right of Way	013059	5.0	5.0	5.0	0.0	4.5	
Capital Outlay	013054	116.0	73.9	73.9	0.0	21.2	
<b>Total South Approach &amp; I/C</b>		<b>143.7</b>	<b>111.3</b>	<b>111.3</b>	<b>0.0</b>	<b>42.7</b>	
<b>Maintenance Facility Phase I &amp; II</b>							(d)
Capital Outlay Support		0.7	4.8	4.8	0.0	3.6	
Capital Right of Way		1.6	1.6	1.6	0.0	1.4	
Capital Outlay		7.0	8.1	8.1	0.0	5.8	
<b>Total Maint. Facility Ph I &amp; II</b>		<b>9.3</b>	<b>14.6</b>	<b>14.6</b>	<b>0.0</b>	<b>10.8</b>	
<b>Demolition – 1927 Bridge</b>							
Capital Outlay Support	01309x	2.0	4.1	4.1	0.0	0.3	
Capital Right of Way	013099	0.0	0.0	0.0	0.0	0.0	
Capital Outlay	013094	16.0	16.0	16.0	0.0	0.0	
<b>Total Demo – 1927 Bridge</b>		<b>18.0</b>	<b>20.1</b>	<b>20.1</b>	<b>0.0</b>	<b>0.3</b>	

Notes

- (a) Includes EA 00601\*, 00608\*, 0060A\*, 0060C\*, 0060F\*, 0060G\*, 0060H\*
- (b) Includes EA 00453\*, 01301\*, 01303\*, 01304\*
- (c) Includes EA 01302\*, 01305\*, 04700\*
- (d) Includes EA 00607\*, 01308\*

## BAY AREA TOLL AUTHORITY

## Appendix C – Project Cost Summary Details (cont'd)

	EA Number	Baseline Budget Jun-00	Current Budget Apr-02	Current Forecast Apr-02	Net Change	Expended to Date (7/98 – 02/02)	Note
<b>Project 3002 - Carquinez Bridge Replacement (cont'd)</b>							
<b>Other Budgeted Capital</b>							(e)
Capital Outlay Support		0.6	7.7	7.7	0.0	1.1	
Capital Right of Way		0.0	1.5	1.5	0.0	0.0	
Capital Outlay		10.6	13.6	13.6	0.0	5.8	
<b>Total Other Budgeted Capital</b>		<b>11.2</b>	<b>22.8</b>	<b>22.8</b>	<b>0.0</b>	<b>7.0</b>	
Total Capital Outlay Support		43.7	96.8	96.8	0.0	56.4	
Total Capital Right of Way		9.6	11.1	11.1	0.0	8.9	
Total Capital Outlay		363.3	354.8	354.8	0.0	182.2	
Project (BATA) Contingency		16.5	17.1	17.1	0.0		
<b>Total Carquinez Bridge</b>		<b>433.2</b>	<b>479.8</b>	<b>479.8</b>	<b>0.0</b>	<b>247.5</b>	
<b>Project 4003 – Richmond-San Rafael Bridge – West Trestle and Fender Rehabilitation</b>							
Capital Outlay Support	0438Ux	5.4	0.7	0.7	0.0	0.7	(f)
Capital Right of Way	0438U9	0.0	0.0	0.0	0.0	0.0	
Capital Outlay	0438U4	33.9	0.0	0.0	0.0	0.0	
Non-BATA Funding		0.0	34.6	34.6	0.0		
Project (BATA) Contingency		6.1	0.0	0.0	0.0		
<b>Total R-SR Bridge – W. Trestle &amp; Fender Rehabilitation</b>		<b>45.4</b>	<b>35.4</b>	<b>35.4</b>	<b>0.0</b>	<b>0.7</b>	
<b>Project 4002 – Richmond-San Rafael Bridge – Deck Rehabilitation</b>							
Capital Outlay Support	04152x	9.0	5.0	5.0	0.0	0.0	
Capital Right of Way	041529	0.0	0.0	0.0	0.0	0.0	
Capital Outlay	041524	33.0	33.0	33.0	0.0	0.0	
Non-BATA Funding		0.0	4.0	4.0	0.0		
Project (BATA) Contingency		11.4	11.4	11.4	0.0		
<b>Total R-SR Bridge – Deck Rehab</b>		<b>53.4</b>	<b>53.4</b>	<b>53.4</b>	<b>0.0</b>	<b>0.0</b>	
<b>Richmond Parkway (Non-Caltrans)</b>							
Capital Outlay Support	non-Caltrans	0.0	0.0	0.0	0.0	0.0	
Capital Right of Way	non-Caltrans	0.0	0.0	0.0	0.0	0.0	
Capital Outlay	non-Caltrans	5.9	5.9	5.9	0.0	2.4	(g)
<b>Total Richmond Parkway</b>		<b>5.9</b>	<b>5.9</b>	<b>5.9</b>	<b>0.0</b>	<b>2.4</b>	
<b>Total Northern Bridge Group</b>		<b>1123.9</b>	<b>1227.2</b>	<b>1227.2</b>	<b>0.0</b>	<b>340.4</b>	

## Notes

(e) Includes EA 01306\*, 01307\*, 0130A\*, 0130C\*, 0130D\*, 0130F\*, 0130G\*, 0130H\*, 0130J\*, 0130X\*

(f) Includes EA 04382\*, 04383\*, 0438U\*

(g) Total reimbursements made to the City of Richmond for current allocation.

BAY AREA TOLL AUTHORITY

Appendix C – Project Cost Summary Details (cont'd)

	EA Number	Baseline Budget Jun-00	Current Budget Apr-02	Current Forecast Apr-02	Net Change	Expended to Date (7/98 – 02/02)	Note
<b>Southern Bridge Group</b>							
<b>Project 6004 – San Mateo-Hayward Bridge Widening</b>							
<b>Widen Trestle</b>							
Capital Outlay Support	04501x	7.9	14.8	14.8	0.0	11.7	(h)
Capital Right of Way	045019	0.0	0.0	0.0	0.0	0.0	
Capital Outlay	045014	124.8	128.9	128.9	0.0	92.8	
<b>Total Widen Trestle</b>		<b>132.7</b>	<b>143.7</b>	<b>143.7</b>	<b>0.0</b>	<b>104.4</b>	
<b>Widen Roadway</b>							
Capital Outlay Support	04503x	4.3	6.0	6.0	0.0	7.2	
Capital Right of Way	045039	1.0	0.0	0.0	0.0	0.0	
Capital Outlay	045034	29.2	26.0	26.0	0.0	25.0	
<b>Total Widen Roadway</b>		<b>34.5</b>	<b>32.0</b>	<b>32.0</b>	<b>0.0</b>	<b>32.2</b>	
<b>Construct Mini Toll Plaza</b>							
Capital Outlay Support		1.7	3.8	3.8	0.0	1.4	
Capital Right of Way	04502x	0.0	0.0	0.0	0.0	0.0	
Capital Outlay	045029	4.4	6.3	6.3	0.0	2.2	
<b>Total Mini Toll Plaza</b>	045024	<b>6.1</b>	<b>10.1</b>	<b>10.1</b>	<b>0.0</b>	<b>3.7</b>	
<b>Other Budgeted Capital</b>							
Capital Outlay Support		1.6	7.0	7.0	0.0	2.3	(i)
Capital Right of Way		0.5	1.4	1.4	0.0	0.5	
Capital Outlay		8.9	8.9	8.9	0.0	0.0	
<b>Total Other Budgeted Capital</b>		<b>11.0</b>	<b>17.3</b>	<b>17.3</b>	<b>0.0</b>	<b>2.8</b>	
Total Capital Outlay Support		15.5	31.6	31.6	0.0	22.5	
Total Capital Right of Way		1.5	1.4	1.4	0.0	0.5	
Total Capital Outlay		167.3	170.0	170.0	0.0	120.0	
Project (BATA) Contingency		19.3	14.4	14.4	0.0		
<b>Total San Mateo-Hayward Bridge Widening</b>		<b>203.6</b>	<b>217.5</b>	<b>217.5</b>	<b>0.0</b>	<b>143.1</b>	
<b>San Mateo-Hayward Bridge – West Approach Replacement Planting</b>							
Capital Outlay Support	04860x	0.1	0.1	0.1	0.0	0.0	
Capital Right of Way	048609	0.0	0.0	0.0	0.0	0.0	
Capital Outlay	048604	0.2	0.2	0.2	0.0	0.0	
Project (BATA) Contingency		0.1	0.1	0.1	0.0		
<b>Total SM-H Bridge W. Approach Replacement Planting</b>		<b>0.4</b>	<b>0.4</b>	<b>0.4</b>	<b>0.0</b>	<b>0.0</b>	

Notes:

(h) Includes EA 04501\*

(i) Includes EA 00305\*, 04504\*, 04505\*, 04506\*, 04507\*, 04508\*, 04509\*

**BAY AREA TOLL AUTHORITY**

**Appendix C – Project Cost Summary Details (cont'd)**

	EA Number	Baseline Budget Jun-00	Current Budget Apr-02	Current Forecast Apr-02	Net Change	Expended to Date (7/98 – 02/02)	Not
<b>I-880/SR-92 Interchange Improvement</b>							(j)
Capital Outlay Support	23317x	20.8	20.8	24.1	3.3	7.0	
Capital Right of Way	233179	8.0	8.0	0.0	-8.0	0.0	
Capital Outlay	233174	70.3	70.3	94.4	24.1	0.0	
Non-BATA Funding		0.0	10.0	10.0	0.0		
Project (BATA) Contingency		25.1	25.1	5.7	-19.4		
<b>Total I-880/SR-92 I/C Improve.</b>		<b>124.2</b>	<b>134.2</b>	<b>134.2</b>	<b>0.0</b>	<b>7.0</b>	
<b>Dumbarton Bridge West Approach Projects</b>							
<b>US101/University Avenue Interchange Reconstruction (non-Caltrans)</b>							
Capital Outlay Support		0.0	0.0	0.0	0.0	0.0	
Capital Right of Way		0.0	0.0	0.0	0.0	0.0	
Capital Outlay		3.8	3.8	3.8	0.0	3.7	(k)
<b>Total US-101/University Ave.</b>		<b>3.8</b>	<b>3.8</b>	<b>3.8</b>	<b>0.0</b>	<b>3.7</b>	
<b>Bayfront Expressway (SR-84) Widening</b>							(l)
Capital Outlay Support	00487x	4.4	4.7	5.7	0.0	4.0	
Capital Right of Way	004879	1.3	0.1	0.1	0.0	0.0	
Capital Outlay	004874	24.8	26.7	25.3	0.0	0.0	
Project (BATA) Contingency		3.3	2.4	2.7	0.0		
<b>Total Bayfront Expressway (SR-84)</b>		<b>33.8</b>	<b>33.8</b>	<b>33.8</b>	<b>0.0</b>	<b>4.1</b>	
<b>Total Dumbarton Bridge W. Approach</b>		<b>37.6</b>	<b>37.6</b>	<b>37.6</b>	<b>0.0</b>	<b>7.6</b>	
<b>Total Southern Bridge Group</b>		<b>365.7</b>	<b>389.6</b>	<b>389.6</b>	<b>0.0</b>	<b>157.8</b>	

**Notes:**

(j) Includes EA 01601\* and 01602\*

(k) Total reimbursements made to the City of East Palo Alto for current allocations

(l) Includes EA 01511\* and 01512\*

**General Notes:**

1. Capital outlay support includes both Caltrans and Architectural/Engineering (A/E) consultant support.
2. Capital right of way includes acquisition, loss of goodwill, demolition, utilities relocations, relocation assistance, titl costs, and sale of excess lands.
3. Capital outlay includes construction bid items, supplemental construction work, state furnished materials, construction contingencies, and external third party agreements.